

ORDINANCE #2008-01

ADAMS COUNTY ROAD ORDINANCE

AN ORDINANCE FOR ADAMS COUNTY, A POLITICAL SUBDIVISION OF THE STATE OF IDAHO, ESTABLISHING ROAD STANDARDS, ESTABLISHING APPLICABILITY, ESTABLISHING ROAD SERVICE CLASSES AND DEFINING CONDITIONS OF DESIGN AND CONSTRUCTION, SETTING FORTH A TABLE OF REQUIREMENTS AND SUPPLEMENTAL DRAWINGS, PROVIDING SEVERABILITY, REPEALING CONFLICTING ORDINANCES, AND PROVIDING AN EFFECTIVE DATE.

SECTION 1: APPLICABILITY

Public roads, roads developed in conjunction with platting of subdivisions, and existing or future private-common-use within subdivisions as defined in Adams County Ordinance 1981-5, or any successor Ordinance, which regulates subdivision development, shall be subject to the Design and Construction Standards adopted by this Ordinance.

SECTION 2: ROAD SERVICES CLASSES ESTABLISHED AND DEFINED

Standards for design and construction depend on Service Classes of the subject roads. The service classes established by this Ordinance are: Public, Private and Private Driveways.

- A. Public Roads (also County Roads) are those motor vehicle travel ways with right-of-way owned by Adams County or dedicated to public use and maintained by Adams County. Right of ways are a minimum of 60 ' wide, obstructions in the right of ways are considered to be subject to removal at the County's request.
- B. Private Roads are motor vehicle travel ways, which are privately owned but serve to provide access, with their current construction or by possible future extension, to access lots.
- C. Private Driveways are roadways serving less than four dwellings.

SECTION 3: ROAD STANDARDS ADOPTED:

Adopted as Resolution 2008-06

Instrument # 116786

COUNCIL, ADAMS, IDAHO

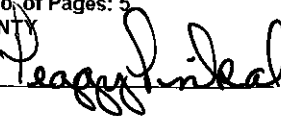
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Recorded for : ADAMS COUNTY

SHERRY WARD Fee: 0.00

Ex-Officio Recorder Deputy

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SECTION 4: DRIVEWAY STANDARDS AND CONDITIONS

- A. Driveways exceeding 250 feet in length shall have a 16-foot wide surface with turnouts every 250 feet (turn out design see drawings). Driveways must terminate in an area that can be used as a turnaround or circular drive. Either a cul-de-sac or hammerhead (see drawing) driveway design will be submitted with building plans. Driveways intersecting County right of way require the same standards as private roads intersecting County rights-of-way.

- B. Driveway Drainage and Grade: At the entrance of any driveway to any road subject to these Standards, there shall be a section of driveway not less than 20 feet long having a grade not steeper than 2% uphill or downhill, to afford drivers of vehicles entering the roadway adequate opportunity to look for traffic before proceeding. Drainage shall not flow onto Public Roadway. Driveways shall not be placed where drivers cannot safely observe traffic on the intersecting road such that entry or exit from the roadway cannot be safely accomplished.

- C. Private Driveways: Surface and base course gravel used on Private Driveways, except that material from specific sources may be approved by the County Road Superintendent without laboratory testing based on the County Road Department's prior experience with material from such sources. Surface course gravel used on Private Driveways shall be granular materials $\frac{3}{4}$ in minus or smaller, 4 inches thick and having an Hveem R-value not less than 80. Pit run base course gravel used on Private Driveways should be granular materials and having a Hveem R-value not less than 55. Base course requirement is waived if native material has a Hveem R-value 55 or greater. Surface course requirement is waived if native material is gradable and has a Hveem R-value 80 or greater, or if native material is sound rock that does not run under traffic nor break during spring thaw. In lieu of determining R-value or experience history of imported base and surface aggregate materials, the pertinent gradation and placement requirements of the Idaho Standards for Public Works Construction, most recent edition, may be applied.

- D. Driveway Grades: Maximum vertical road grades shall not exceed the following:
 - 1. The maximum grades for Private Driveway construction should be 7%.
 - 2. Private Driveways should not exceed 7% in average grade, but may be given permission for grades up to 10% for abnormal construction difficulties such as rock outcrops, provided that

recovery sections with grades not exceeding 7% immediately follow the steep sections and that are approved by the County. Two grades greater than 7% within 1000 ft of each other is the maximum allowed without a variance from the County.

3. To construct a driveway section with grades above 7% the following standards shall be adhered to:
 - a. Grade 10%: A gravel surface shall be a crushed $\frac{3}{4}$ in minus gravel with a minimum of 75% fractures, or equivalent granular material having a Hveem R-value not less than 80.
 - b. Grade 10%: Maximum length of 10% grade shall not exceed 300 feet. Intermediate grade between the 10% critical lengths shall not exceed 7% for a minimum length of 200 feet. Sighting distance along grade shall be a minimum of 200 feet.

SECTION 5: SEVERABILITY:

The provisions of this Ordinance shall be deemed severable. Should any provision of this Ordinance be declared invalid by the Court of competent jurisdiction, the remainder shall continue in full force and effect and shall be interpreted in a manner to effectuate the intent of the Ordinance as a whole.

SECTION 6: RELATIONSHIP TO OTHER ORDINANCES-REPEAL OF CONFLICTING ORDINANCES:

Should any zoning, subdivision, or other Ordinance adopted pursuant to authority granted by Idaho Code Title 67, Chapter 65 or Title 50, Chapter 13, existing as of the effective date of this Ordinance or adopted during the effective duration of this Emergency Ordinance, conflict with the terms of this Ordinance, such conflicting provisions of said other Ordinance are hereby repealed and or invalidated to the extent of such conflict. A subsequently adopted Ordinance may supersede the provisions of this Ordinance if it expressly states that such is its intent.

SECTION 7: VARIANCE AUTHORIZED – STANDARD:

- A. A variance from the express standards established by this Ordinance may be allowed in circumstances where unique site characteristics cause practical difficulties and unnecessary hardships in complying with said standards and when variance from those standards can be accomplished without jeopardizing the public safety or compromising the purpose of this Ordinance.
- B. A request for variance shall accompany the application to establish a road as required by the Road Standards Resolution. Said application

shall be completed by the owner or the owner's agent and shall set forth with particularity the specific variance sought, the specific physical conditions which prompt the request for the variance, and the engineering alternatives which have been considered in order to attempt compliance with the Road Standards Resolution. The Adams County Building Administrator and Fire Chief of said District will confer and offer a decision on the variance.

- C. A notice of any request for variance shall be provided by first class mail to adjoining property owners and to the Fire Chief providing at least 10 (10) days advance notice of the meeting at which the request for variance will be heard. Such Notice will explain the nature of the variance sought and the specific legal standard, which would be varied thereby. Variance applicants shall pay the application fee established by the Board of Commissioners by Resolution and shall submit such documentation as deemed necessary by the Administrator. After hearing from the applicant, any interested parties, and conducting such other independent investigation as the Administrator deems appropriate, the Administrator shall act upon the request for variance and may approve any such variance only if it can find affirmatively:
 - 1. That the variance is necessitated by unique site characteristics, which make compliance with the Ordinance and Road Standards impractical and/or unnecessarily difficult.
 - 2. That the requested variance will not adversely affect the public health, safety and welfare or otherwise substantially impair the property interest of adjoining property owners.
 - 3. That the variance requested cannot be accomplished through implementation of any feasible engineering alternative. Mere cost differential is not evidence of infeasibility.
- D. The decision of the Administrator shall be in writing, shall set forth the reasons therefore, and shall be delivered to the Adams County Board of Commissioners for approval or denial.
- E. Upon a variance request denial, the applicant may appeal to the Board of County Commissioners by providing notice and a statement of the grounds there for within 28 days of the decision of the Administrator, and payment of such application fee as the Board of Commissioners may establish by Resolution.
- F. On the appeal, the Board of Commissioners may at upon the record compiled before the Administrator, remand the matter to the Administrator for further proceedings, or it may conduct its own hearing. Any decision of the Board shall be in writing and shall set

forth the reasons therefore. The audio record of the appellate hearing shall be retained for one (1) year from the date of hearing.

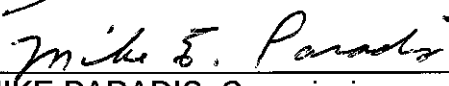
SECTION 8: EFFECTIVE DATE:


This Ordinance shall be effective upon its passage and publication according to law.

APPROVED as an Ordinance of Adams County, Idaho, on the 9th day of June 2008.

**BOARD OF COUNTY COMMISSIONERS
ADAMS COUNTY, IDAHO**


BILL BROWN, Chairman


MIKE PARADIS, Commissioner


JOE HOLMES, Commissioner

ATTEST:


SHERRY WARD, Clerk of the Board

RESOLUTION 2008-06

WHEREAS, on the 6th day of June, 2008, at the Adams County Courthouse in Council, Idaho; the Board of County Commissioners adopted THE FOLLOWING RESOLUTION WAS UNANIMOUSLY ADOPTED, TO-WIT:

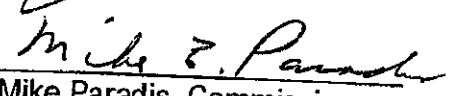
ADAMS COUNTY does hereby adopt the enclosed Minimum Standards for Road Design and Construction.

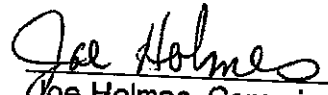
NOTICE IS HEREBY GIVEN, That the Board of County Commissioners of Adams County, Idaho.

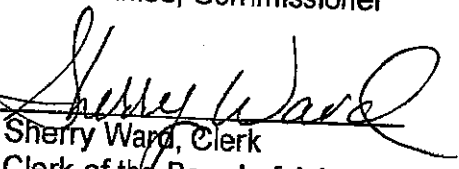
APPROVED AND ADOPTED this 9th day of June, 2008.

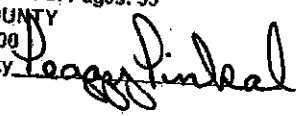
Adams County Board of County Commissioners


Bill Brown, Chairman


Mike Paradis, Commissioner


Joe Holmes, Commissioner

Attest: 
Sherry Ward, Clerk
Clerk of the Board of Adams County Commissioners

Instrument # 116785
COUNCIL, ADAMS, IDAHO
7-22-2008 09:43:46 No. of Pages: 35
Recorded for : ADAMS COUNTY
SHERRY WARD Fee: 0.00
Ex-Officio Recorder Deputy 
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**ADAMS COUNTY
MINIMUM STANDARDS FOR
ROAD DESIGN AND CONSTRUCTION**

May, 2008



**ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION**

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**ADAMS COUNTY
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ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION

Definition of Terms

- **AASHTO** - American Association of State Highway and Transportation Officials.
- **Alley** – Public access of limited use intended only to provide access to the rear or side of lots or buildings in urban districts.
- **Applicant** - The person or persons making application to Adams County, to obtain a permit for grading, road construction, utility installation or driveway access.
- **Arterial Highway** – Highway controlled and maintained by the Idaho Transportation Department.
- **Best Management Practices (BMP)** - A measure or combination of measures determined to be the most effective and practical means of preventing or reducing contamination to ground water and/or surface water from nonpoint and point sources to achieve water quality goals and protect the beneficial uses of the water.
- **Cul-de-sac Road** – Local road having one end permanently terminated in a vehicle turnaround.
- **Dedication** – The setting apart of land or interest in land for use by the public. Land becomes dedicated when accepted by Adams County as a public dedication, either by ordinance, resolution, entry in the official minutes, or by the recording of a plat showing such dedication.
- **DU** – Dwelling Unit
- **Easement** – Grant of the right to use a strip of land for a specified purpose.
- **Engineer** – Professional Engineer registered in the State of Idaho.
- **Frontage Road** – Minor road parallel and adjacent to a major route that intercepts local traffic and controls access to the major route.
- **Highway** – Roadway designated as a State or Federal Highway by the responsible state or federal agency.
- **Irrigation Facilities** – Canals, laterals, ditches, conduits, gates, wells pumps, and equipment necessary for the supply, delivery and drainage of irrigation water.

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- **ISPWC** – Idaho Standards for Public Works Construction.
- **ITD** – Idaho Transportation Department.
- **KSF** – 1000 square feet.
- **Local Road** – Road that provides direct access to residential, commercial, and/or industrial sites for local traffic movements and connects to minor and major roads or arterial highways.
- **Loop Road** – Minor road with both terminal points on the same road of origin.
- **Major Collector Road** – General term for a road including primary county roads that provide travel corridors between cities, recreational sites and industrial areas.
- **Minor Collector Road** – Roadway that provides for traffic movement within neighborhoods and between major roads and local roads with occasional access to abutting property.
- **MUTCD** – Manual on Uniform Traffic Control Devices.
- **Plat** – Map of a subdivision:
 - **Preliminary Plat** – Preliminary plan, subdivision or dedication containing the elements and requirements set forth in Article II, Section 210 and 215 of the Adams County Land Use and Regulations Plan
 - **Final Plat** – Plan of the plat, subdivision or dedication, or any portion thereof, prepared for recording by the Adams County Recorder.
- **Private Road** – Road within a subdivision plat that is not dedicated to the public and not a part of the public road system.
- **Private Road Right-of-Way** – Privately owned land for a private road system.
- **Public Highway Agency** – This term includes the Adams County Road Department, the Idaho Transportation Department and the functioning Public Works departments of the Cities of Council, New Meadows and Indian Valley.

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- **Public Right-of-Way** – Any land dedicated and open to the public and under the jurisdiction of a public highway agency.
- **Public Road** – Road, thoroughfare, alley, highway or bridge under the jurisdiction of a public highway agency.
- **Reserve Strip** – Strip of land between a dedicated road or partial road and adjacent property, in either case, reserved or held in public ownership for future road extension or widening.
- **Roadway** – That portion of a highway improved, designed or ordinarily used for vehicular travel, exclusive of sidewalks, shoulders, berms, and other portions of the public right-of-way.
- **Specification** – The construction specifications contained in the latest edition of the Idaho Standards for Public Works Construction (ISPWC) as modified by Adams County.
- **Traveled Way** – The portion of the roadway for the movement of vehicles exclusive of ditches and roadside areas.
- **Utility Facilities** – Installations or equipment, underground or overhead, furnished for use by the public, including but not limited to: electricity, gas, steam, television, communications, water, drainage, irrigation, sewage disposal, or flood control, owned and operated by any person, firm, corporation, municipal department, or board duly authorized by state or municipal regulations.

Variances

Variations from these standards must be approved by the Adams County Planning and Zoning Commission and the Adams County Board of County Commissioners. Variations will be reviewed on an individual basis.

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I. DESIGN CRITERIA

A. GENERAL DESIGN CRITERIA

1. The following standard guidelines have been adopted by reference:
 - a. Roadway Design – AASHTO, A Policy on Geometric Design of Highways and Streets; AASHTO Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT≤400)
 - b. Signs and Markings – MUTCD, Manual on Uniform Traffic Control Devices.
 - c. Roadside Safety – AASHTO, Roadside Design Guide
 - d. Bridges – AASHTO, LRFD Bridge Design Specifications.
 - e. Road Structure – Asphalt Institute, Design Guide and Traffic Index.
 - f. Drainage – ITD Design Manual
 - g. Traffic – TRB, Highway Capacity Manual and ITE, Trip Generation Manual
 - h. Environmental – BMP Handbook Best Management Practices for Idaho Rural Road Maintenance..
 - i. Mailboxes – LHTAC Manual for the Location, Support and Mounting of Mailboxes
 - j. Utilities – LHTAC Manual for the Use of Public Right-of-Way Permits for Utilities and Encroachments
 - k. Construction Specifications - Idaho Standards for Public Works Construction (ISPWC)

Where possible, all designs shall be based on these guidelines and the applicable design criteria set forth therein. Variation from these design guidelines shall be based on site specific conditions, sound engineering judgment, and consideration of the safety of the traveling public.

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B. ROADWAY CLASSIFICATION

1. All roadways are classified in accordance with the Federal Highway Administration guidelines. All roads are classified as principal arterials, minor arterials, major collectors, minor collectors or local roads. Roadway classifications are based on existing and future traffic volumes and adjacent land use patterns. It shall be the prerogative of the Adams County Board of Commissioners to define the classification and level of maintenance for roads on the County road system.
2. Private Roads shall meet all Local Road standards

C. DESIGN SPEED

1. The minimum design speeds shall conform to the design criteria listed in Table 1 on design traffic volumes; higher design speeds may be required.

Table 1. Minimum Design Speeds

Roadway Classification	Level	Mountainous
Local Road	25	15
Collector	35	30
Arterial	50	40

D. ROAD RIGHT-OF-WAY

1. The minimum public right-of-way width for each roadway classification is shown in Table 2. Additional right-of-way and/or permanent easements may be required to accommodate snow storage and cut or fill slopes. The minimum public right-of-way width applies only to new construction after the date of adoption of this Road Standard.

Table 2. Minimum Right-of-Way Widths

Type of Roadway	Minimum Right-of-Way Width
Principal Arterial	120 feet
Minor Arterial	80 feet
Principal and Minor Collectors	60 feet
Local Roads	60 feet

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2. Cul-de-sacs shall have a minimum right-of-way of a 60-foot radius with additional right-of-way as needed to accommodate snow storage and cut or fill slopes. Cul-de-sacs with a different shape and configuration may be allowed, providing that adequate public right-of-way is provided, and the proposed geometry shall accommodate a WB-50 design vehicle. The design shall be approved by Adams County. A standard cul-de-sac layout is shown in Figure 150.
3. All right-of-way lines at road and highway intersections and at cul-de-sac bulbs shall be connected by a curve having a minimum radius of twenty feet (20') or a chamfer of forty feet (40').

E. ALIGNMENT

1. Table is intended to show the minimum and maximum values for specific roadway design criteria. Design criteria for items not listed shall conform to AASHTO policy. Modification of the design criteria may be allowed on an individual project basis.

Table 3. Geometric Design Criteria

Design Parameter	Arterial	Collector	Local Road
Vertical Grades ¹	Minimum 0.5% Maximum 7%	Minimum 0.5% Maximum 7%	Minimum 0.5% Maximum 7% ²
Super Elevations	Max 0.06 ft. per foot	Max 0.06 ft. per foot	Max. 0.04 ft. per foot
Angles of Intersection	80 - 90°	80 - 90°	70 - 90°

¹Roadways constructed using curb and gutter sections require a minimum grade of 0.3%

²Maximum Length of 10% grade shall not exceed 300 feet. Intermediate grade between the 10% critical lengths shall not exceed 7% for a minimum length of 200 feet. Sighting distance along grade shall be a minimum of if 200 feet.

F. ROADWAY CROSS-SECTION

1. The typical roadway sections are shown in the attached figures. These guidelines show the cross-section characteristics required for public roads in Adams County.
 Figure 100. Gravel Local Road
 Figure 110. Paved Local Road
2. Roadways with curb, gutter, and sidewalk will be reviewed on an individual basis.
3. New irrigation facilities shall be constructed and maintained outside the public right-of-way.

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G. STRUCTURAL ROAD SECTION

1. An adequate base and surface thickness is required for all roads. Structural road section calculations shall follow the Asphalt Institute design guidelines (equation listed below) and be submitted to Adams County for review.

$$T = 0.0032*(TI)*(100-R), \text{ where}$$

T = total gravel equivalent (ft.)
TI = traffic index
R = "R-value" of subgrade material

H. CUT AND FILL SLOPES

1. Except where a Soils and Geology Report by a licensed Professional Engineer or Professional Geologist stipulates that materials on a specific site will be stable at steeper slopes, shall be as follows:
 - a. For sections where the cut as measured from the uphill shoulder is less than or equal to 10 feet vertically, cut slope shall not be steeper than 1:1.
 - b. For sections where the cut as measured from the uphill shoulder is greater than 10 feet vertically, cut slope shall not be steeper than 1.5:1.
2. Where a Soils and Geology Report by a licensed Professional Engineer or Professional Geologist identifies native materials at road locations, cut slopes shall be as recommended by such report but not steeper than the following:
 - a. in Solid Rock requiring blasting, 0.5:1,
 - b. in Jointed Rock removable by ripping, 0.75:1,
 - c. in Naturally Cemented or Bonded Material, 1:1, or
 - d. in Loose Material, 1.5:1.
3. Fill slopes shall be not steeper than 1.5:1.

I. APPROACHES

1. Any applicant planning to construct an approach to access the public right-of-way for any purpose shall obtain an approved "Application and Permit to Use Right-of-Way (Approaches)."

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2. See Figure 300. No work of any nature shall be performed on public right-of-way until an approved permit has been issued. In an emergency, approval may be given in advance of processing the permit.
3. Application fees for various types of public right-of-way use permits issued by Adams County shall be established by resolution.
4. Adams County may request a traffic impact study be completed prior to granting an approach permit if the proposed development will generate over 50 cars per hour.
5. No part of the public right-of-way shall be used for:
 - a. the parking of vehicles except in authorized parking areas.
 - b. the servicing, refueling, repairing, of vehicles except for emergencies
- b. displays, sales, exhibits, business signs etc.
- c.
6. Approaches shall be located so as not to create undue interference with, or hazard to, the free movement of normal roadway or pedestrian traffic or cause areas of congestion. Approaches must be located where the roadway alignment and profile are favorable, i.e., away from sharp curves, steep grades, and/or where the sight distance would not be adequate for safe traffic operations. Approach locations that restrict or interfere with the placement and proper functioning of traffic control signs, signals, lighting, or other devices must also be avoided.
7. Approaches shall be constructed to provide the sight distances defined in the AASHTO, A Policy on Geometric Designs of Highways and Streets
8. Approaches shall be graded to drain away from the roadway.
9. Not more than 2 approaches shall access any single property.
10. Approaches shall be wide enough to properly serve the anticipated type and volume of traffic. Minimum widths should be used only when space limitations must be considered. Table 4 indicates standard approach widths.

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Table 4. Standard Approach Widths

	Minimum	Maximum
Residential	Twelve feet (12')	Thirty feet (30')
Agricultural	Twelve feet (12')	Forty feet (40')
Commercial (one-way)	Fifteen feet (15')	Thirty feet (30')
Commercial (two-way)	Twenty feet (20')	Forty feet (40')
Street/Highway	Twenty-eight feet (28')	Forty-eight feet (48')

11. The construction of Joint Use Approaches is encouraged. A joint-use approach should use the maximum dimensions of a single approach.
12. Approaches in speed zones that are over 35 mph shall be at least twenty (20) feet wide minimum.
13. Approaches shall be constructed in conformance with the driveway plan shown on Figure 200.
14. Minimum approach spacing is shown in Figure 210. Approaches shall be located as far as possible from intersections to:
 - a. Preserve visibility at the intersection
 - b. Allow a vehicle that is leaving the approach to enter the desired traffic lane before entering the intersection
 - c. Permit a vehicle crossing the intersection to enter the approach in an orderly, safe manner with a minimum of interference to through traffic
 - d. Facilitate the installation of traffic signs, signals, and lighting where required
15. Roads and driveways accessing paved public roads shall be paved from the edge of the public roadway to the edge of the Right of Way.

J. MAILBOX TURN OUTS

1. No mailbox or newspaper delivery box (mailbox) will be allowed to exist on the County's rights-of-way if it interferes with the safety of the traveling public or the function, maintenance, or operation of the road system.

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2. The location and construction of mailboxes shall conform to the rules and regulations of the U.S. Postal Service, as well as, the Manual for the Location, Support and Mounting of Mailboxes adopted by the County.
3. A mailbox installation that conforms with the Local Highway Technical Assistance Council Manual for the Location, Support and Mounting of Mailboxes (Mailbox Manual) will be considered acceptable. Mailbox location, height, support, mounting and turnout construction shall be in accordance with the Mailbox Manual.
4. Any exceptions to the Mailbox Manual may be granted if in the judgment of the County, the installation does not interfere with the safety of the traveling public or the function, maintenance, or the operation of the street system. Requests for any exception to the Mailbox Manual shall be in writing. The request shall contain sufficient details to evaluate the requested exception.
5. Mailbox turnouts shall be located and constructed as shown in Figure 220 and Figure 230.

5. Location

- a. It will be the responsibility of the postal patron to inform the County of any new or existing mailbox installation where turnout construction is inadequate to permit all-weather access to the mailbox. An all-weather turnout is sufficiently stable to support passenger cars stopping regularly during all weather conditions. The sub-base (ballast), base and surface treatment is a requirement of all new or reconstructed turnouts, and shall be the same as the adjacent roadway section.
- b. No mailbox will be permitted where access is obtained from the lanes of an arterial or where access is otherwise prohibited by law or regulation. Where a mailbox is installed in the vicinity of an existing guardrail, it should, whenever practical, be placed behind the guardrail.
- c. Exceptions to the lateral placement criteria may exist on certain designated rural roads where the County deems it is in the public's interest to permit lesser clearances or to require greater clearances.

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6. Mailboxes

Mailboxes shall be of light metal or plastic construction, conforming to the requirements of the U.S. Postal Service. Newspaper delivery boxes shall be of light sheet metal or plastic construction of minimum dimensions suitable for holding a newspaper.

7. Supports

Mailbox supports shall not be set in concrete, unless the support design has been shown to be safe by crash tests when so installed. A metal post shall not be fitted with an anchor plate, but it may have an anti-twist device (stabilizer fin) that extends no more than 10 inches below the ground surface.

8. Mounting

The post-to-box attachment details should be of sufficient strength to prevent the box from separating from the post if the installation is struck by a vehicle. The product must result in a satisfactory attachment of the mailbox to the post, and all components must fit together properly.

9. Removal of Non-conforming or Unsafe Mailboxes

Any mailbox that is found to violate the intent of this regulation shall be removed by the postal patron upon notification by the County. At the discretion of the County, based on an assessment of hazard to the public, the patron will be granted not less than 24 hours or more than 15 days to remove an unacceptable mailbox. After the specified removal period has expired, the unacceptable mailbox will be removed by the County at the postal patron's expense.

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K. DRAINAGE

1. All drainage systems for areas larger than 16 acres shall be designed by a Professional Engineer licensed in the State of Idaho. The following table may be used to size culverts for areas up to 16 acres.

TABLE FOR CULVERT SIZING

Watershed Area (acres)	Required Culvert Diameter, Inches	Culvert Capacity Cubic Ft. per Sec.
5 to 8	18	6
8 to 15	24	12

2. Downstream drainage systems shall not be adversely affected by upstream development. It is the developer's responsibility to ensure the runoff from a development does not contain pollutants and that the volumes and flow rates do not exceed pre-development conditions.
3. Drainage designs shall be based on a precipitation event with a return period of twenty-five (25) years for local roads and fifty (50) years for collector roads. Major Collectors, bridges, and primary storm conveyance runs shall be designed to accommodate a 100 year event.
4. Section 600.00 – Hydraulics of the Idaho Transportation Department (ITD) Design Manual is recommended for estimating the volume of runoff.
5. Culverts used for drainage purposes shall be corrugated steel, aluminum with the wall thickness and minimum depth of cover shown in Table (other culvert materials may be used if approved by Adams County):

Table 4. Culvert Materials

Diameter Inches	Steel Thickness Inches	Aluminum Thickness Inches	Cover Required *
18" through 36"	0.051 (16 ga)	0.075 (14 ga)	12" minimum

*Cover may be reduced to six inches (6") on residential driveways with 12 gage steel pipe.

6. Design flows shall not exceed 80% of pipe capacity.

ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION

7. The preferred minimum pipe diameter shall be 18 inches for culverts under roadways until the culvert length of 70 feet is reached. The minimum diameter for culverts less than 70 feet in length may be reduced to 12 inches if site conditions prevent the required cover with an 18 inch pipe and a 12 inch culvert satisfies the hydraulic requirements of Section K.1. All culverts over 70 feet long shall be 24 inches or more in diameter. Minimum pipe diameter for culverts under driveways and approach roads shall be 18 inches. The minimum diameter of pipe for storm sewers, siphons, and irrigation systems shall be 18 inches. Pipe carrying drainage from irrigated lands shall be considered as culverts and the appropriate minimum size used.
8. Culverts under driveways shall be installed as shown on the Figure 200.
9. All necessary drainage easements for accommodating drainage structures shall be shown on the plans and dedicated as a part of the approved plat. Drainage easements necessary for conveying storm water across private property shall be shown on the plat and recorded with Adams County.

L. WATER QUALITY

1. Adams County has adopted the BMP Handbook Best Management Practices for Idaho Rural Road Maintenance to assist local agencies and developers with the selection, design, installation and maintenance of BMPs to reduce storm water pollution. The handbook presents general guidelines to mitigate water quality impacts of new construction.
2. Road construction and developments must meet all state and federal requirements.

ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION

M. STRUCTURES

1. Bridge structures shall be designed by a Professional Engineer licensed in the State of Idaho, in accordance with AASHTO LRFD Bridge Design Specifications, latest edition.
2. The minimum design vehicle for bridge construction on local and minor collector roads shall be an HS-20 truck. Arterial and Major Collector roads shall be designed for an HS-25 truck.
3. The minimum width of the bridge structure measured face-to-face of curb or the face of the bridge rails shall be the full width of the traveled way, plus six feet (6').
4. All structural retaining walls shall be designed by a Professional Engineer licensed in the State of Idaho and shall be approved by Adams County prior to construction.

N. SIGNING

1. All traffic control devices (signs, pavement markings, and markers) shall be shown on the roadway design plans.
2. The traffic control devices and their application shall conform to the Manual on Uniform Traffic Control Devices (MUTCD), latest edition.
3. Traffic control signs and private signs shall comply with Adams County sign standards detailed in the Adams County Land Use and Development Ordinance.
4. All signs shall be installed prior to the acceptance of roads, unless approved otherwise by Adams County.
5. Adams County may determine pavement-marking standards subject to MUTCD requirements. The color, pattern and dimensions of marking shall be in conformance with the MUTCD, latest edition. Paint quality shall be the same as that used by the Idaho Transportation Department for their pavement markings.
6. All temporary and construction traffic control shall conform to the MUTCD, latest edition.

**ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION**

O. TRAFFIC

1. Traffic volumes from new developments shall be determined using the ITE – Trip Generation Manual. The following rates shown in Table 8 are recommended for general traffic estimation.

Table 5. Common Trip Generation Rates

Land Use	Unit	Average Daily Traffic Vehicle Per Day
Residential	DU	8.0 vpd
Retail	KSF	40.0 vpd
Industrial	KSF	10.0 vpd

DU = Dwelling Unit
KSF = 1000 Square Feet

2. Roadway capacities shall be evaluated according to the recommendation of the Transportation Research Board (TRB) – Highway Capacity Manual. A level of service rating of C or better is required for all county roads.
3. Auxiliary lanes shall be provided according to AASHTO guidelines.

P. DESIGN VEHICLE

1. All public roads shall be designed to accommodate an intermediate semi-trailer (WB-50) with an outside wheel path radius of 45 ft.
2. All private roads shall be designed to accommodate a single unit (SU) fire truck (WB-50) with an outside wheel path radius of 42 ft.

Q. UTILITIES AND RIGHT-OF-WAY ENCROACHMENTS

1. All new utility installations, existing utility installations to be retained, relocated, maintained or adjusted because of roadway construction or reconstruction, and utilities to be relocated because they constitute a definite hazard to the traveling public on all public right-of-way under Adams County jurisdiction, shall comply with the LHTAC Manual for the Use of Public Right-of-Way Permits for Utilities and Encroachments

ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION

2. Each new utility installation or encroachment which is to occupy public right-of-way shall require a permit. Existing utility facilities or encroachments which are to be relocated or adjusted to a Position within the public right-of-way of a construction project shall require a permit. A permit is also required for utility facilities not adjusted, but which remain in place with the public right-of-way of a construction project. Utility facilities not adjusted and already covered by permit will not require a new permit. Maintenance projects for seal coats, pavement overlays, pavement rehabilitation's, etc., will not require a permit for utility facilities not adjusted.

3. When required or indicated on the permit, the permittee shall furnish a surety bond in the amount specified in the special provisions of the permit for installation and future repair, relocation or removal of the facilities. Not work shall be commenced under the permit until the said bond has been submitted and approved. The bond will be returned after the Adams County has given written approval of the utility facility.

R. WARRANTY

1. Construction of all newly constructed roads adopted by Adams County shall be warranted for 2 years to be free of defect. All roads shall be paved, then chip sealed prior to adoption by Adams County.

ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION

CONSTRUCTION SPECIFICATIONS

S. DIVISION 100 – GENERAL CONDITIONS

The Idaho Standards for Public Works Construction (ISPWC) (as amended or modified herein) shall control road construction work in Adams County.

1. Road, drainage and utility improvements are required by Adams County as a condition of approved land use. All improvements (public and private) shall be completed prior to the recording of the final plat or financially guaranteed as provided in the Adams County Subdivision Regulations.
2. All testing required in these standards or required by Adams County shall be done by an accredited or approved testing laboratory at the expense of the applicant or contractor. Copies of all tests shall be submitted to Adams County for review.
3. All plans, submittals, calculations, reports and materials shall be in English units.
4. Changes to any materials, quality control or workmanship on public improvement projects shall be approved by Adams County in writing.
5. Adams County shall be notified at least five business days prior to start of construction

T. DIVISION 200 - EARTHWORK

1. In solid rock excavation, the solid rock shall be excavated to six inches (6") below the finished subgrade elevation and back-filled with approved granular materials.
2. Unstable sub-grade conditions shall be remedied by over-excavation and back-filling with approved granular material. Geotextile material may be required.
3. Class A compaction shall be specified for construction

**ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION**

U. DIVISION 300 – TRENCHING

1. A right-of-way use permit shall be obtained from Adams County prior to commencing work in any public right-of-way.

V. DIVISION 700 – CONCRETE

1. The basic mix design for curb, gutter and sidewalk work on County roads shall have at least a 28-day strength of 3000 lbs. (Class 30).

W. DIVISION 800 - UNCRUSHED AGGREGATE

1. Pit run material shall be durable, have a sand equivalent not less than 30, and shall meet the gradation shown in **Table**.

Table 6. Sub-base Gradation (% Passing)

Sieve Size	Subbase
8"	95-100
6"	
2"	
#4	15-65
#200	0-12

X. DIVISION 800 - CRUSHED AGGREGATE

1. The crushed aggregate for base shall conform to **Table**.

Table 7. Base Material Gradation (% Passing)

Sieve Size	% Passing
1"	100
3/4"	90-100
#4	40-65
#8	30-50
#200	3-9

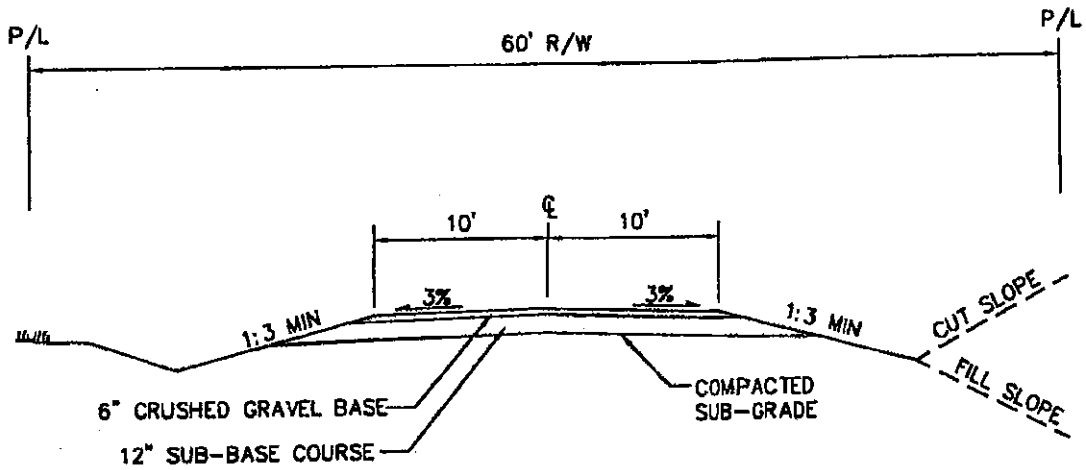
2. Material shall be mechanically compacted by rolling to at least ninety-five percent (95%) of the AASHTO T-99 Proctor Density.

**ADAMS COUNTY
MINIMUM STANDARDS FOR ROAD DESIGN AND CONSTRUCTION**

Y. DIVISION 800 - ASPHALT PAVING

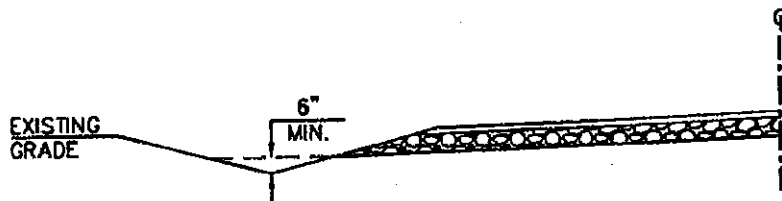
- a. Asphalt pavement shall meet Idaho State Department of Transportation Standards, Class I plant mix. The asphalt cement performance grade shall be selected for the site temperature requirements.

FIGURES



NOTE: SECTION THICKNESS BASED ON T₁=4, SUBGRADE R=5, AND ADAMS COUNTY MINIMUM REQUIREMENTS.

GRAVEL ROAD SECTION

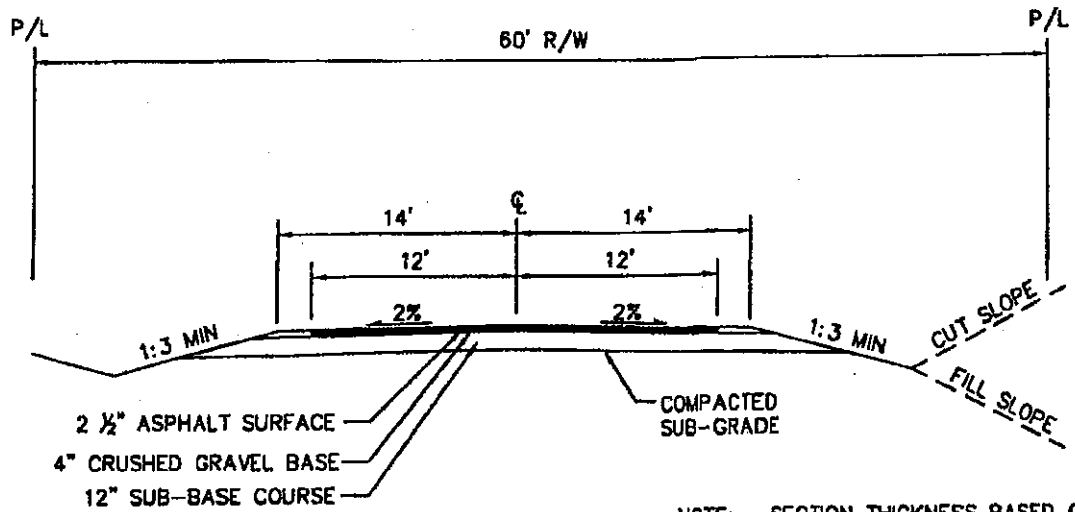


ROADSIDE DITCH DETAIL
(TYPICAL FOR ALL ROAD CLASSIFICATIONS)

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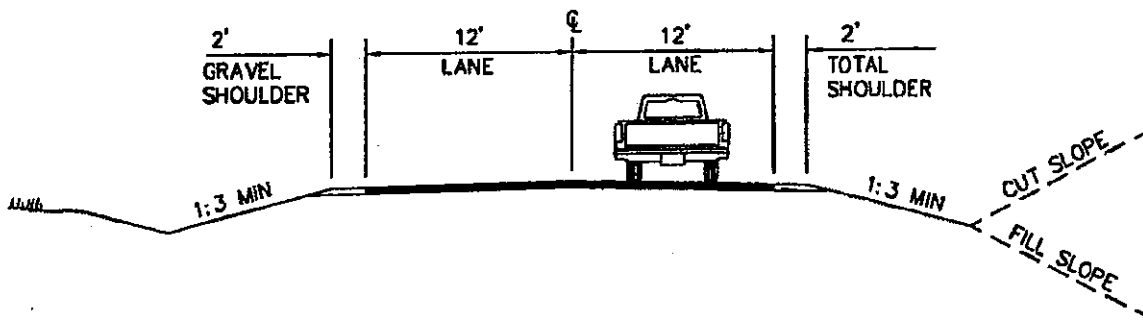
ADAMS COUNTY ROAD DEPARTMENT	GRAVEL LOCAL ROAD	FIGURE NO. 100
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NOTE: SECTION THICKNESS BASED ON T=4, SUBGRADE R=5, AND ADAMS COUNTY MINIMUM REQUIREMENTS.

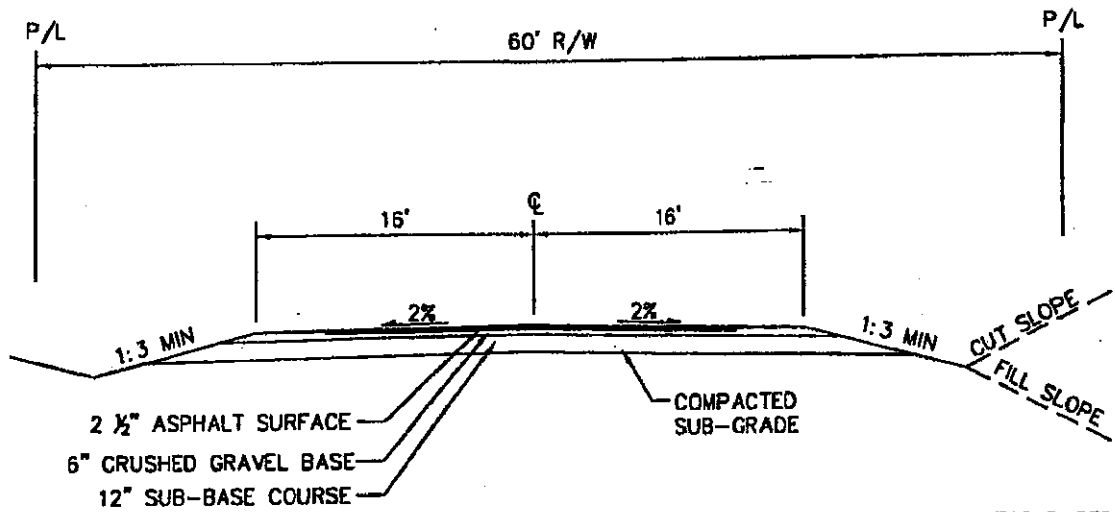
MINIMUM STRUCTURAL SECTION



LOCAL ROAD DIMENSIONS

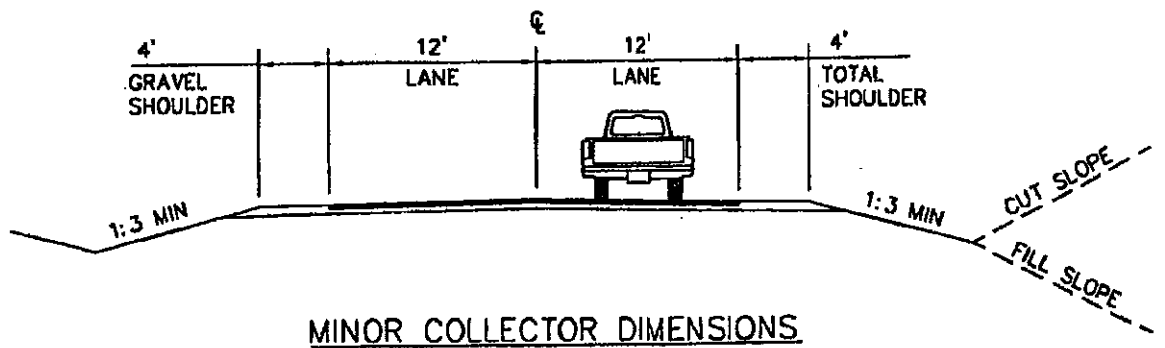
ADAMS COUNTY ROAD DEPARTMENT	PAVED LOCAL ROAD	FIGURE NO. 110
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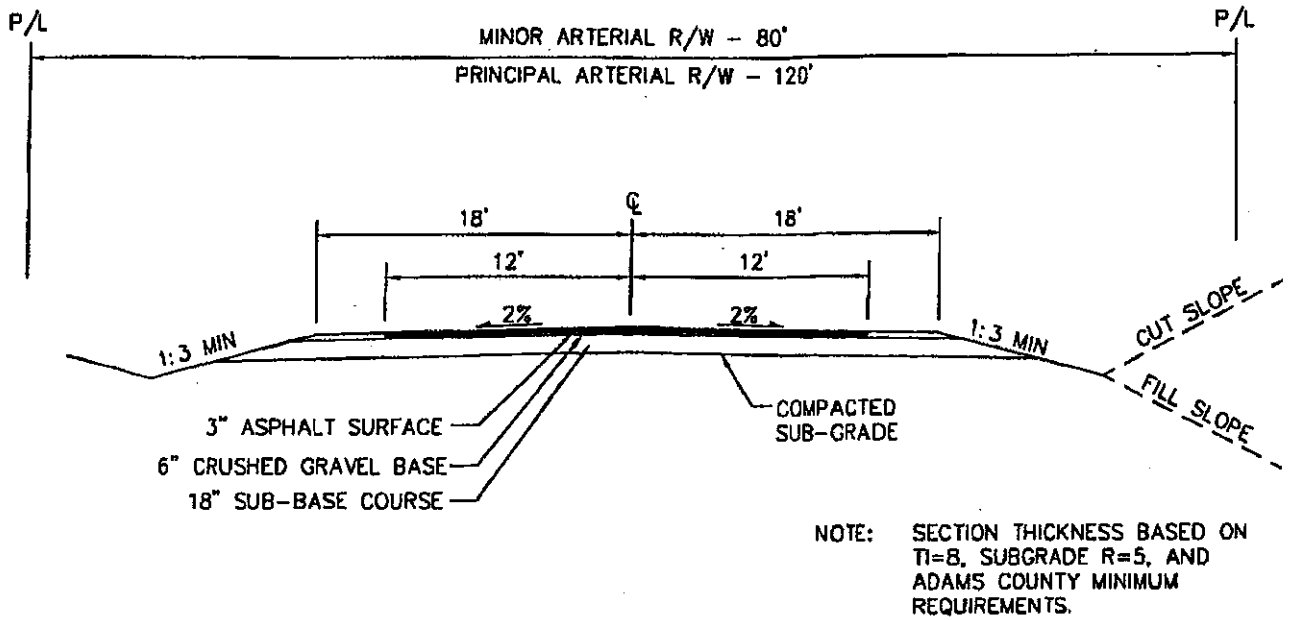
NOTE: SECTION THICKNESS BASED ON $T_1=6$, SUBGRADE $R=5$, AND ADAMS COUNTY MINIMUM REQUIREMENTS.

MINIMUM STRUCTURAL SECTION

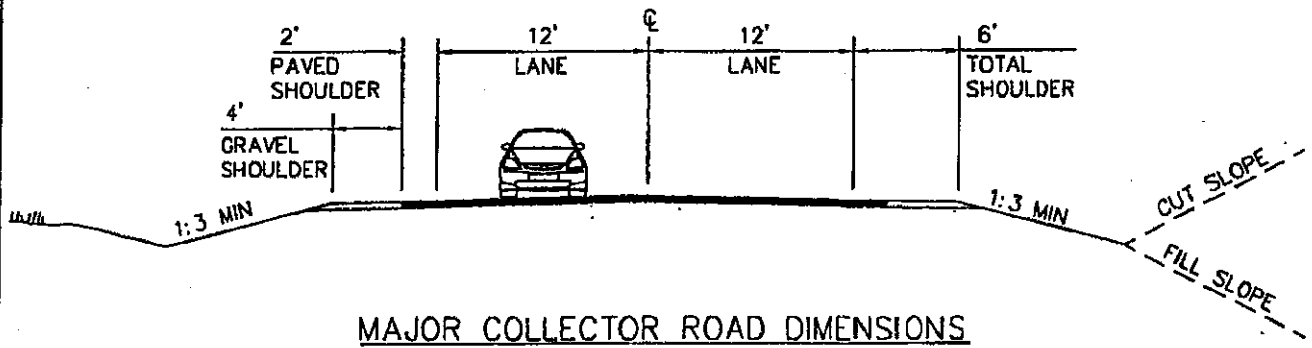


MINOR COLLECTOR DIMENSIONS

ADAMS COUNTY ROAD DEPARTMENT	COLLECTOR ROAD	FIGURE NO. 120
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MINIMUM STRUCTURAL SECTION

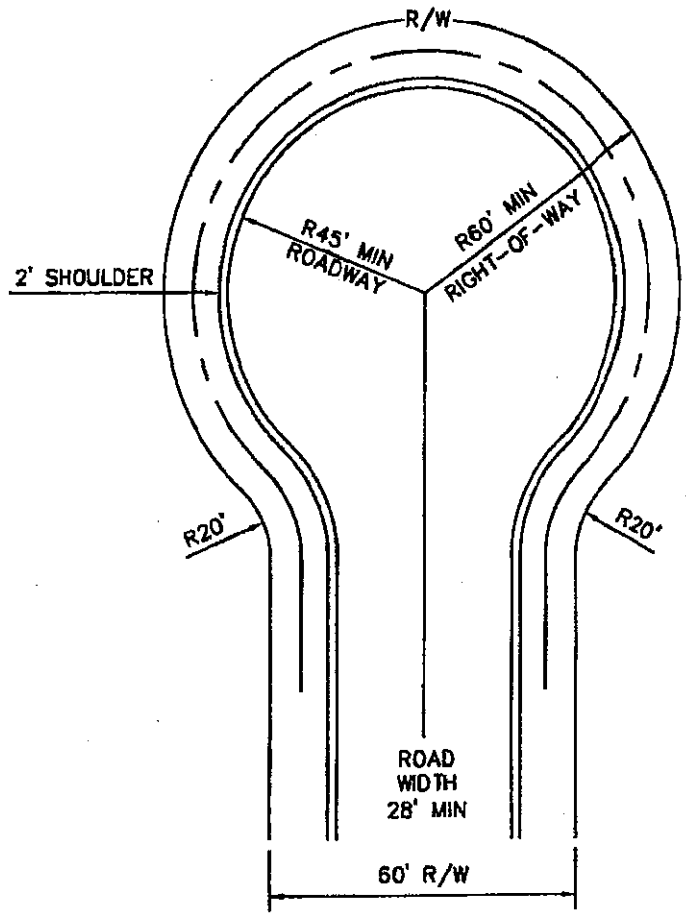


MAJOR COLLECTOR ROAD DIMENSIONS

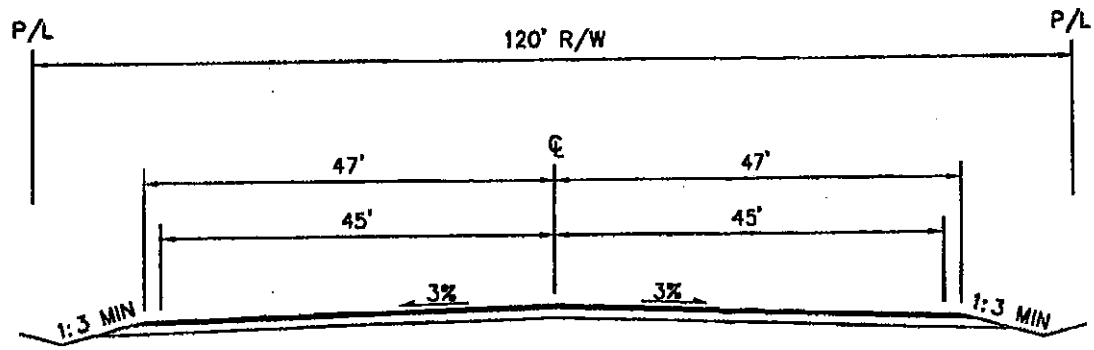
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ADAMS COUNTY ROAD DEPARTMENT	ARTERIAL ROAD	FIGURE NO. 130
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STANDARD DIMENSIONS

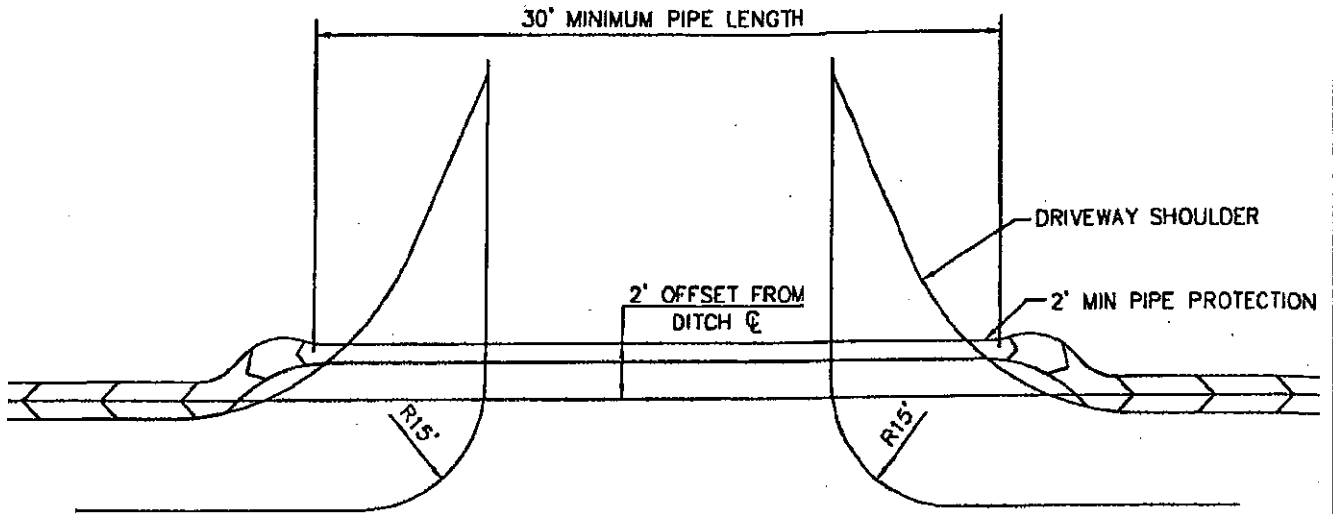


NOTE: ROADWAY AND SHOULDER WIDTHS ALSO APPLY TO LOCAL ROADS.

CUL-DE-SAC SECTION

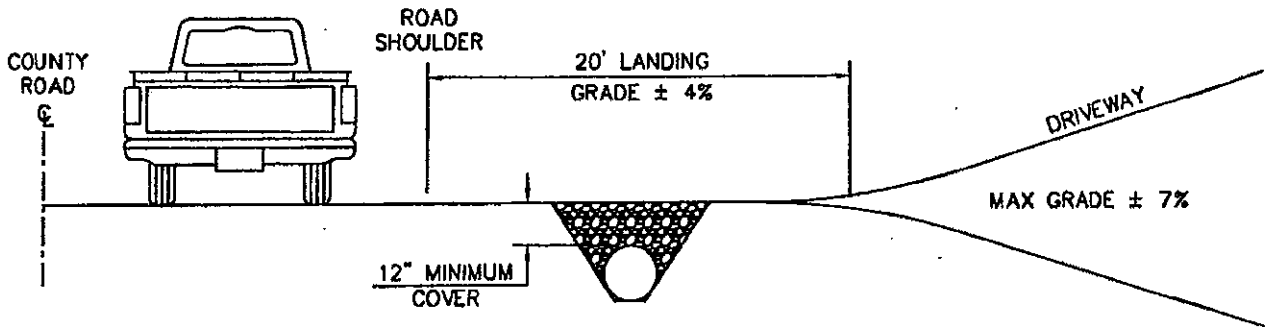
<p>ADAMS COUNTY ROAD DEPARTMENT</p>	<p>STANDARD CUL-DE-SAC</p>	<p>FIGURE NO. 150</p>
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DRIVE WIDTH VARIES



COUNTY ROAD

PLAN VIEW



RECOMMENDED DRIVEWAY PROFILE

NOTE: APPROVED MATERIAL
STEEL CMP16 GAUGE
RIBBED POLYETHYLENE
ALUMINUM 14 GAUGE.

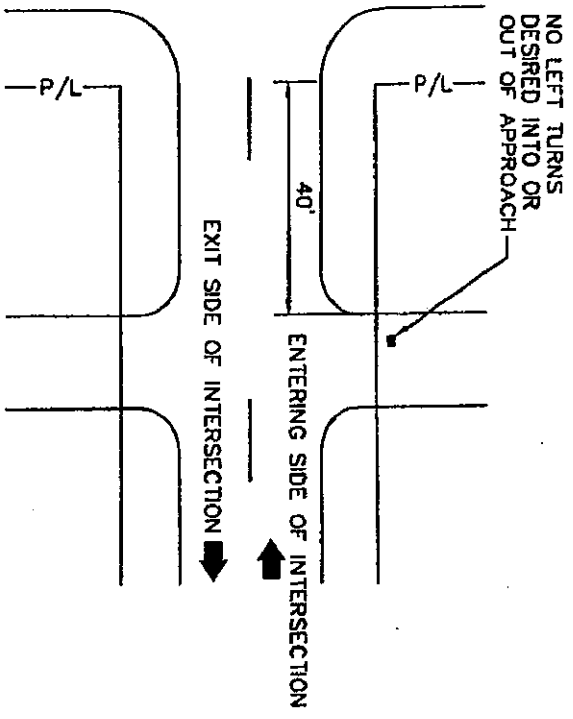
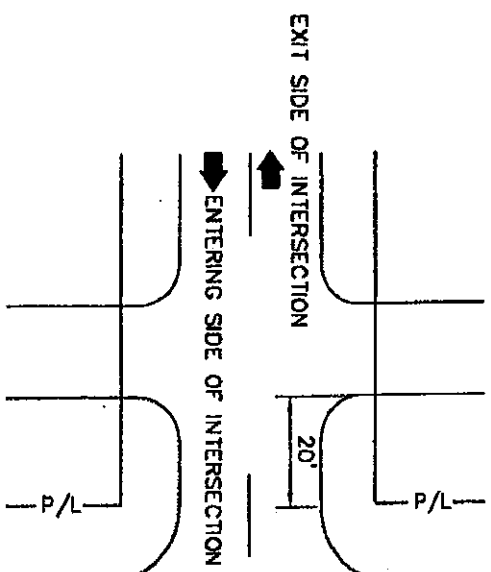
NOTE: 6" FOR 12 GAUGE STEEL PIPE

ADAMS COUNTY
ROAD DEPARTMENT

DRIVEWAY PLAN

FIGURE NO.

200



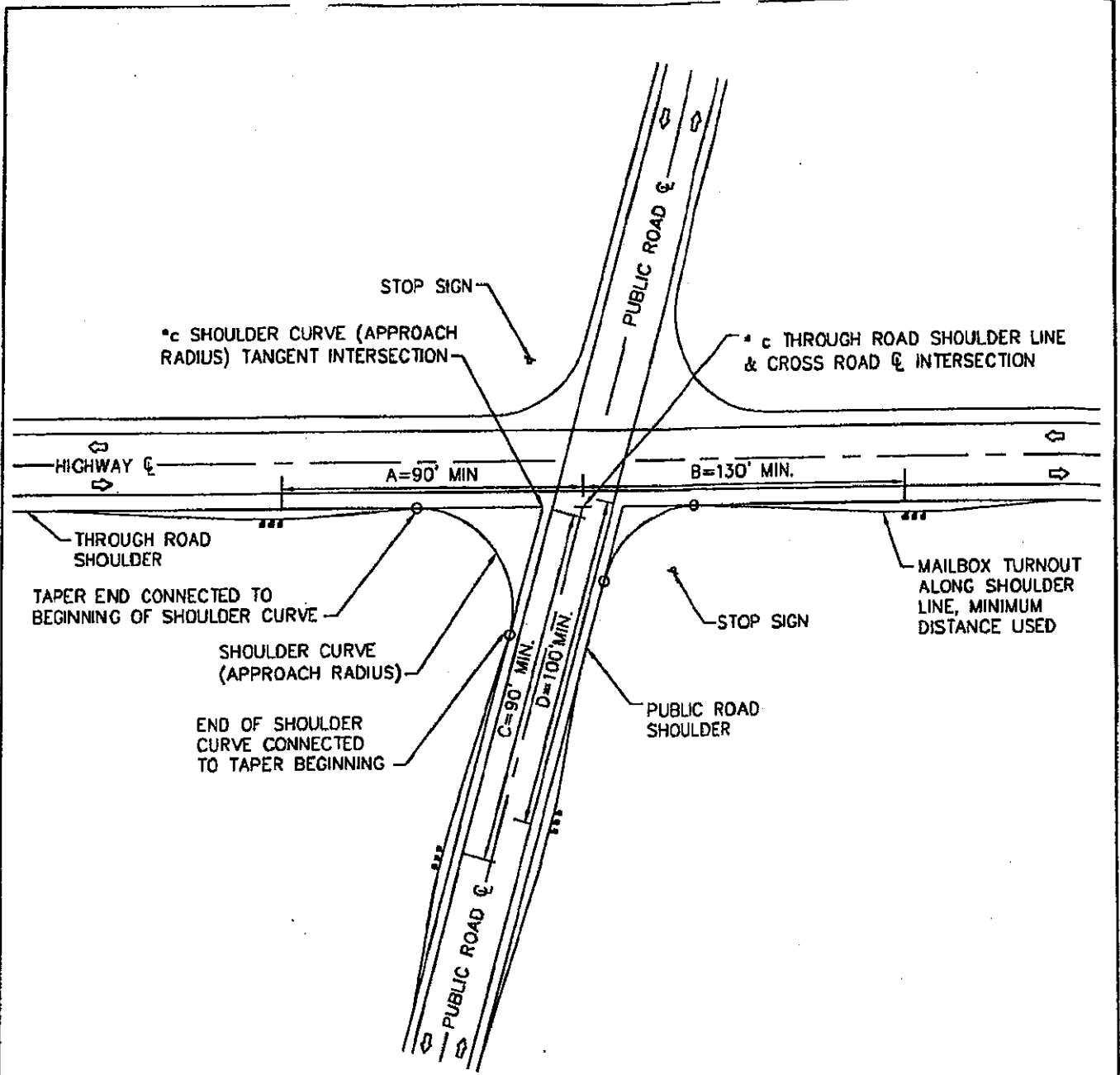
CORNER CLEARANCE DIAGRAM
WITHOUT CURB AND GUTTER

ADAMS COUNTY ROAD DEPARTMENT

CORNER CLEARANCE DIAGRAM

FIGURE NO.

210



*c SEE NOTE NO. 1

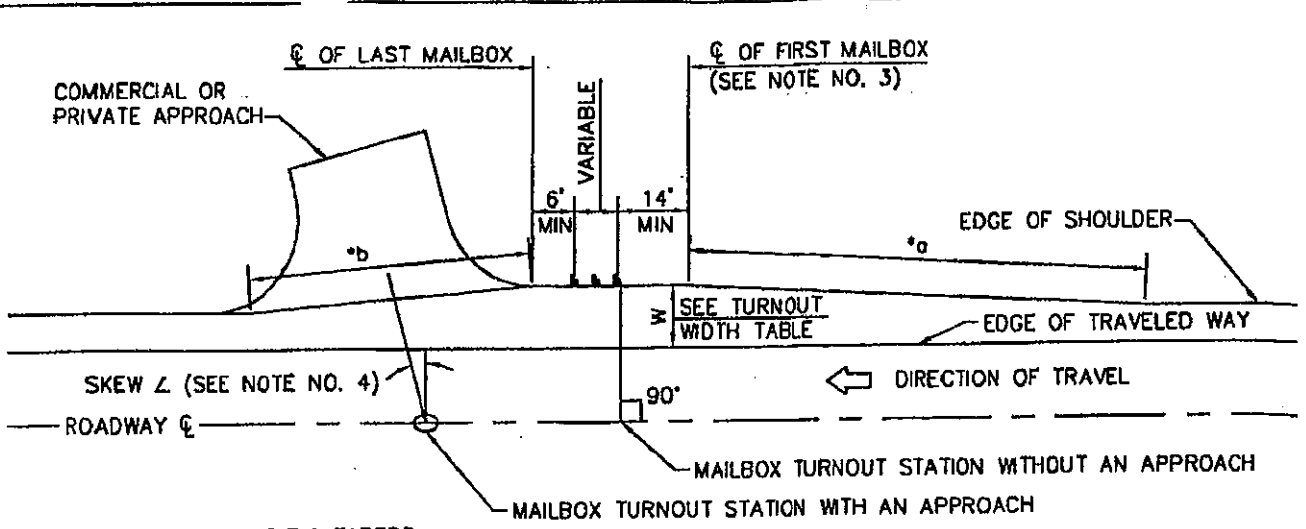
NOTES

1. EITHER THE FRONT OR BACK TAPER END OF A MAILBOX TURNOUT MAY BE CONNECTED TO THE SHOULDER CURVE (APPROACH RADIUS) P.C. OR P.T. OTHERWISE THE TURNOUT SHALL BE SHIFTED ALONG THE ROADWAY SHOULDER TO MEET THE MINIMUM DISTANCE REQUIREMENT. GENERALLY, WHEN A STANDARD MAILBOX TURNOUT IS USED IN THE ABOVE MANNER, THE DISTANCE A,B,C, AND D WILL EXCEED THE MINIMUMS SHOWN.

MAILBOX TURNOUT LOCATION AT PUBLIC ROAD INTERSECTIONS

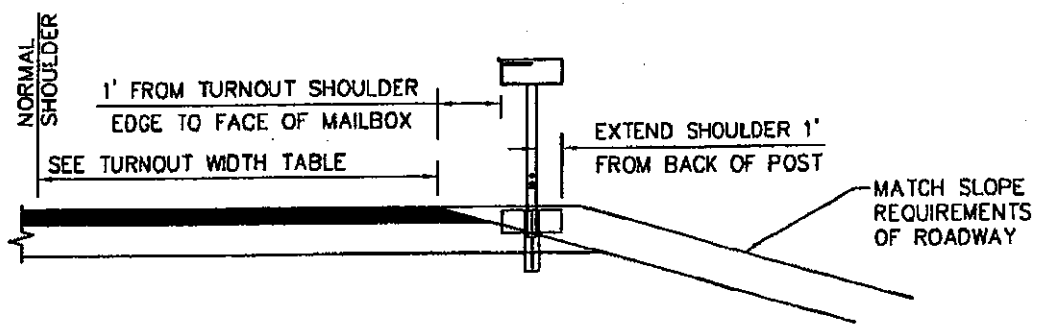
ADAMS COUNTY ROAD DEPARTMENT	MAILBOX TURNOUT AT INTERSECTIONS	FIGURE NO. 220
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 COUNTY OF ADAMS ROAD DEPARTMENT



*a = 4:1 & b = 2.5:1 TAPERS:
 FOR ROADS WITH SPEEDS OF 40 MPH OR LESS, OR AN ADT OF 400 OR LESS.
 *a = 20:1 & *b = 12:1 TAPERS:
 FOR ROADS WITH SPEEDS GREATER THAN 40 MPH OR AN ADT GREATER THAN 400.

MAILBOX TURNOUT
 NTS



MAILBOX SLOPE TYPICAL
 NTS

NOTES

2. WHEN USING THE TURNOUT WIDTH TABLE THE "SPEED" (MPH) IS THE POSTED SPEED LIMIT. THE "ADT" IS THE AVERAGE DAILY TRAFFIC.

TURNOUT WIDTH TABLE *(SEE NOTE NO. 2)		
SPEED/ADT	PREFERRED	MINIMUM
55/>10000	>12'	12'
55/>1500-10000	12'	10'
55/100-1500	10'	8'
55/<100	8'	6'
<40/<50	6'	4'

- ONLY COMMERCIAL AND PRIVATE APPROACHES SHALL QUALIFY TO HAVE MAILBOX TURNOUTS INSTALLED ADJACENT TO AND AS PART OF THE APPROACH. FOR PUBLIC ROAD APPROACHES AND INTERSECTIONS THE LOCATION METHOD AS SHOWN ON THE "MAILBOX TURNOUT LOCATION - INTERSECTIONS" SHALL BE USED (SEE SECTION IV A). WHEN MAILBOX TURNOUTS ARE USED AT PUBLIC ROAD INTERSECTIONS, MEASUREMENTS SHALL BE MADE TO/FROM THE FIRST MAILBOX CENTERLINE, PARALLEL TO THE ROADWAY CENTERLINE.
- MAILBOX TURNOUTS SHALL NOT BE SKEWED. THE APPROACH RADIUS MUST BLEND FROM THE ROADWAY SHOULDER TO THE TURNOUT SHOULDER AS SHOWN IN THE "MAILBOX TURNOUT" DETAIL.
- THE BALLAST (SEE DEFINITION OF TERMS) REQUIREMENTS OF MAILBOX TURNOUTS SHALL BE AS THE ADJACENT ROADWAY SECTION.

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General Provisions

1. A deposit in an amount to be determined (typically 120% of the estimated construction cost) by Adams County (minimum \$200) shall accompany this application. If proper construction or repair is made and accepted within ten (10) days, the deposit will be refunded. If proper construction or repair is not completed within (10) days, the Adams County may make repairs and assess the deposit. A \$75.00 administrative fee is non-refundable.
2. Adams County may change, amend or terminate this permit or any of the conditions herein enumerated if permittee fails to comply with its provisions or requirements as set forth herein.
3. Approaches shall be for the bona fide purpose of securing access and not for the purpose of securing access and not for the purpose of parking, conducting business, or servicing vehicles on the public right-of-way.
4. No revisions or addition shall be made to an approach or its appurtenances on the public right-of-way without the written permission of Adams County.
5. The permittee shall furnish all material, labor and equipment involved in the construction of the approach and its appurtenances. This shall include furnishing approved drainage pipe of a size specified on permit (12 inch minimum) curb and gutter, concrete sidewalk, etc., where required. Materials and workmanship shall be good quality and are subject to inspection and approval by Adams County.
6. Adams County reserves the right to require the permittee, its successors and assign, at any time, to make such changes, additions, repairs and relocations to any approach or its appurtenances within the public right-of-way as may be necessary to permit the relocation, reconstruction, widening, drainage, and maintenance of the roadway and/or to provide proper protection to life and property on or adjacent to the roadway.
7. Approaches shall conform to the plans made a part of this permit. Adequate drawings or sketchings shall be included showing the design, materials, construction requirements and proposed location of the approach. All approaches shall be in accordance with Adams County minimum standards for road design and construction.
8. During the construction of the approach (es), such barricades, sign and other traffic control devices shall be erected and maintained by the permittee, as may be deemed necessary by Adams County. Said devices shall conform to the current issue of the Manual on Uniform Traffic Control Devices. Parked equipment and stored materials shall be as far from the traveled way as feasible. Items stored within 30 feet of the traveled way shall be marked and protected. Adams County may provide barricades (when available) upon request.
9. In accepting this permit, the permittee, its successors and assign, agrees to hold Adams County harmless from any liability caused by the installation, construction, maintenance or operation of the approach (es).
10. If the work done under this permit interferes in any way with the drainage of the roadway, the permittee shall wholly and at his own expense make such provision as Adams County may direct to take care of said drainage problem.
11. Upon completion of said work herein contemplated, all rubbish and debris shall be immediately removed and the roadway and roadside shall be left neat and presentable and to the satisfaction of Adams County.
12. The permittee shall maintain at his or their sole expense the structure or object for which this permit is granted in a condition satisfactory to Adams County.
13. Neither the acceptance of this permit nor anything herein contained shall be construed as a waiver by the permittee of any rights given it by the constitution or laws of the state of Idaho or of the United States.
14. No work shall be started until an authorized representative of Adams County has given written notice to the permittee to proceed, except in case of an emergency when verbal authorization may be given with a written permit and fee required within five (5) working days.
15. This permit shall be void unless the work herein contemplated shall have been completed before _____.